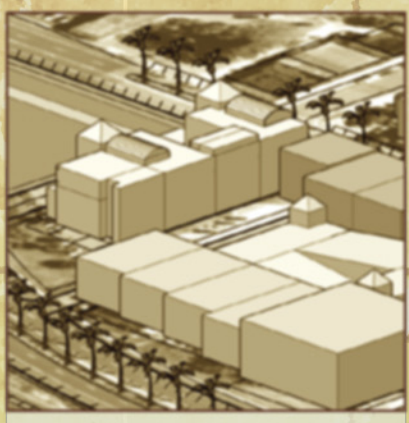


NEWSPAPER CITY 500



VISUAL FOCAL POINT



BLUEPRINT BRUNSWICK



# OGLETHORPE BLOCK

DESIGN STANDARDS



August 2006



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# Introduction

## A. Introduction

This document presents standards for the design of the several public and private elements of the redevelopment plan for the “Oglethorpe Block” in Downtown Brunswick. The standards are based on a previous study, The Oglethorpe Block Study, done by this consultant team which recommended a preferred development plan and program for this block along with strategies for phasing and developing the project. The conceptual development has been accepted by the City of Brunswick and Glynn, both of whom own properties which constitute the Oglethorpe redevelopment block (see figure 1, which situates the block in the context of Downtown Brunswick). The redevelopment is to be coordinated by the non-profit “Blueprint Brunswick” for whom this study is also prepared. Blueprint Brunswick is the outgrowth of the Brunswick Downtown Redevelopment Plan, which first identified the Oglethorpe Block as a prime “catalyst site” for igniting downtown’s resurgence.

The Oglethorpe Block also falls wholly within the Brunswick Historic District, administered by the Brunswick Historic Review Board and governed by local ordinance and supplemental Historic Preservation Design Guidelines. Also, discussions for the inclusion of Shopfront Building Standards in the district regulations are currently taking place, the latest draft of which is included in the Appendix. Together, these four existing documents form the legal basis for design of the Oglethorpe Block and are hereby appended to this document.

This document is meant to create the additional configuration and basic design requirements for the unique elements proposed for this block. These additional requirements are intended to be conveyed as covenant restrictions along with any lot sales or leases to private owners. It is recommended that all requests by property owners for variances and amendments to these covenant restrictions should be submitted to the Historic Review Board for approval following the current procedures found in the Historic District ordinance.

## B. Project Overview

The Oglethorpe Block consists of two city blocks in downtown Brunswick, Georgia. The block is bordered by Newcastle Street to the east, Bay Street to the west and F Street to the south. Bay Street turns toward Newcastle Street, forming the northern boundary of the site. G Street currently intersects the block. A passive park occupies the northern part of the block, north of G Street. The County Health Services building and the historic Penny’s retail building occupied the southern part of the block until 2005, when the County Health building was demolished to prepare the block for redevelopment. The Penny’s building and adjacent parking remain privately owned and operated.





**1** Context Map  
Not to Scale





**2** Keymap: Area of Influence  
Not to Scale



The opportunity to locate a small community conference center in the Oglethorpe block was obvious to civic leaders. The Chamber of Commerce has outgrown its building on Highway 17 and is interested in moving into the downtown core. The possibility of locating a civic building that would house the conference center and the Chamber of Commerce became appealing. This building and its associated civic spaces could be located at the northern end of the Oglethorpe Block and would serve as a gateway feature into downtown. The rest of the block could be re-developed in stages to maximize opportunities as they became available.

Several key components for the Oglethorpe Block were outlined in the Blueprint Brunswick study, completed in early 2003. As one of the final 'catalyst' sites, the Oglethorpe Block is seen as a key opportunity to stimulate development downtown and on the waterfront.

The design of the block's redevelopment is specified in detail in the 2004 "Oglethorpe Block Study". The study divides the block into two main areas; The "Civic Zone" and the "Commercial Zone". The main design features can be summarized as follows:

### 1. "Civic Zone"

The Civic Zone consists of the proposed conference center, the Chamber Group office building and all associated open spaces and infrastructure. There are six main components, all of which constitute an integrated and functional whole. They are:

- *The Executive Conference Center*

The Center is planned as a small conference facility designed to accommodate the City and County's unmet conference and event demand. It is seen as a stand-alone facility that is connected to the Chamber Group's building and operations and potentially to proposed Phase 2 private development, which could include a small hotel. It consists primarily of a multi-use room which could be divided into smaller spaces along with the necessary pre-function and support functions, including a catering kitchen.

- *The "Chamber Group" Office Building and Visitors Center*

The Chamber Group, consisting of the Chamber of Commerce, the Convention and Visitors Bureau, and the Glynn County Development Authority, would be housed in a separate building which would be connected to the Conference Center. The building would house the executive offices of the three organizations, a "walk-in" visitor's center for the City, County and region, and the substantial processing and mailing operations of the Convention and Visitors Bureau.

- *Shared Spaces*

The Civic complex is configured with both indoor and outdoor spaces forming a "bridge" or link



between the Conference Center and Office Building. These spaces contain functions which could be shared by both facilities, reducing the need to build duplicate facilities. They include such spaces as:

- a) A covered entry portico connecting the main entries to both facilities.
- b) An outdoor courtyard, which could be used as an event or pre-function space by either facility as a separate rental space for private events.
- c) Medium sized conference rooms which could be used by either facility.

- *Oglethorpe Park*

The existing triangle park would be redesigned and rebuilt as a major outdoor civic venue with components such as:

- a) A “gateway” monument feature at the point of the triangle, which is the historic and symbolic entry point into downtown from the north.
- b) A “civic plaza”, which could function as a gathering space for public events, as well as a rental space associated with the Conference Center.
- c) Smaller and quieter spaces for small groups, which could be used as occasional outdoor “break-out” spaces associated with the Conference Center.

Redevelopment of this park would depend on the proposed reconfiguration of Bay Street and the intersection of Bay and Newcastle Streets and therefore would have to be part of future phases of development of the block.

## 2. Commercial Zone

The Oglethorpe Block will be redeveloped to accommodate a mix of uses between F Street and G Street and open space will occupy space north of G Street. Civic uses will dominate the northern end of the block while providing a direct connection between the residential neighborhoods to the east, the historic Newcastle Street corridor to the south and the future waterfront developments to the west. Private development will occur over two later phases and will front Newcastle Street and Bay Street. Several components are necessary to facilitate a successful redevelopment not only on the block, but in the surrounding areas. These components are:

- *G Street Realignment*

The first component of the block strategy is to realign G Street to correspond with the historic alignment of the G Street right-of-way through the Oglethorpe Block in keeping with the historic grid of downtown Brunswick. This provides better connectivity from downtown to future waterfront developments and (The realignment) also allows more space for redevelopment on the retail/mixed-use portion of the Oglethorpe Block. Moving G Street also provides the opportunity to reconfigure the existing park to serve as a gateway to historic downtown and as an outdoor meeting space for the proposed conference center and other public functions. Civic-scaled



street treatments including streetlights, trees, benches, plantings, and other pedestrian amenities should be provided on G Street. The travel lanes should be designed for slow vehicular traffic through the area and should allow for drop-off at the civic buildings. This can be emphasized with cobblestones or brick paving in the street.

- *Bay Street*

Bay Street is seen as a key opportunity to be modified to suit a more urban setting. Current discussions and studies suggest that Bay Street may have excess capacity and could be reconfigured to allow better traffic control and on-street parking. (Concerns over making Bay Street too narrow for future development and the fact that it is both a State and Federal Route resulted in the proposed narrowing of Bay Street to four lanes, two in each direction. By eliminating two lanes of traffic, there is room for on-street parking and a landscaped median. This configuration is more conducive to quality downtown development and is more in keeping with the scale and character of Newcastle Street. Also, the division between the downtown retail core and the waterfront is lessened significantly by eliminating two lanes of traffic and providing on-street parking. The spaces provided on the street will partially satisfy parking needs while reinforcing a downtown atmosphere. A total of 60 on-street parking spaces will be available on the Oglethorpe Block of Bay Street.). Such improvements to Bay Street would enhance the development potential of the Oglethorpe Block and provide additional needed parking, especially for anticipated public events.

- *Newcastle Street*

Newcastle Street has a charming character that complements the scale and the uses in the historic downtown. It is suggested that the same treatments that make the street successful be continued along the length of the Oglethorpe Block. This will include on-street parking, wide sidewalks, plantings, and a median. An additional 30 on-street parking spaces will be provided with this treatment.

### 3. Parking Strategy

Parking is a key concern to any downtown or retail destination. Since the redevelopment of the Oglethorpe Block will include a mix of uses such as conference spaces, office space, storefront retail, upper story residential, and possibly even a hotel, parking needs will fluctuate with each phase that is constructed. Times of day, build-out uses, and the success of the conference center will all influence parking demand. It is assumed that daily parking needs created by the redevelopment of the Oglethorpe Block will be satisfied within the block and by the new on-street parking. Additional on-street parking around the block can also provide space for visitors to the downtown. A phased lot/deck strategy is suggested to accommodate daily users of the block.





The parking strategy involves phasing parking on the interior of the Oglethorpe Block. A surface lot is proposed behind the first-phase civic uses and in the middle of the Oglethorpe Block. The surface lot will contain as many as 60 parking spaces and will be funded as part of the conference center and Chamber of Commerce building. This is more than sufficient for the daily needs of the civic building and the first phase of private development along Newcastle Street.

When the development facing Bay Street is constructed, that developer can expand the garage by adding one or possibly two more floors, depending on the use of the space. This will provide an additional 60 to 120 parking spaces inside the Oglethorpe Block. In this manner, each potential user finances parking that will be required to support its use.

#### 4. The Service Corridor

A substantial service corridor is planned in rear of the Civic complex off of Bay Street which can serve the needs of the Conference Center and the Chamber Group operations, in particular the mailing operations of the Convention and Visitors Bureau, as well as the Newcastle Street commercial block and the Bay Street mixed use block. This corridor would be built in Phase 1 of the development and should be designed to accommodate both phase 1 and future service needs and parking needs of the development of the rest of the block. The service corridor falls wholly within the current G Street public right of way.

#### 5. Phasing Strategy

Successful phasing will be important to the eventual completion of the Oglethorpe Block redevelopment. The Civic Zone will be funded by a combination of public and private sources. The Conference Center and site work will be funded by public funds and the Chamber Group office building will be funded privately. In Phase One, the land will be prepared for redevelopment, including the realignment of G Street and the phase one surface parking lot. This investment in downtown will catalyze development both on the waterfront and along the Oglethorpe Block of Newcastle Street. That development will set the stage for future development of the Bay Street frontage of the block. The land that is privately owned (the Penny's Building and its associated parking lot) will be improved and/or expanded as the owner sees a demonstrated demand. The timing for each phase will be market driven.

These design concepts are illustrated in the following “functional” and “urban design” diagrams from the “Oglethorpe Block Study”.



# Site Regulating Plans

## Overview

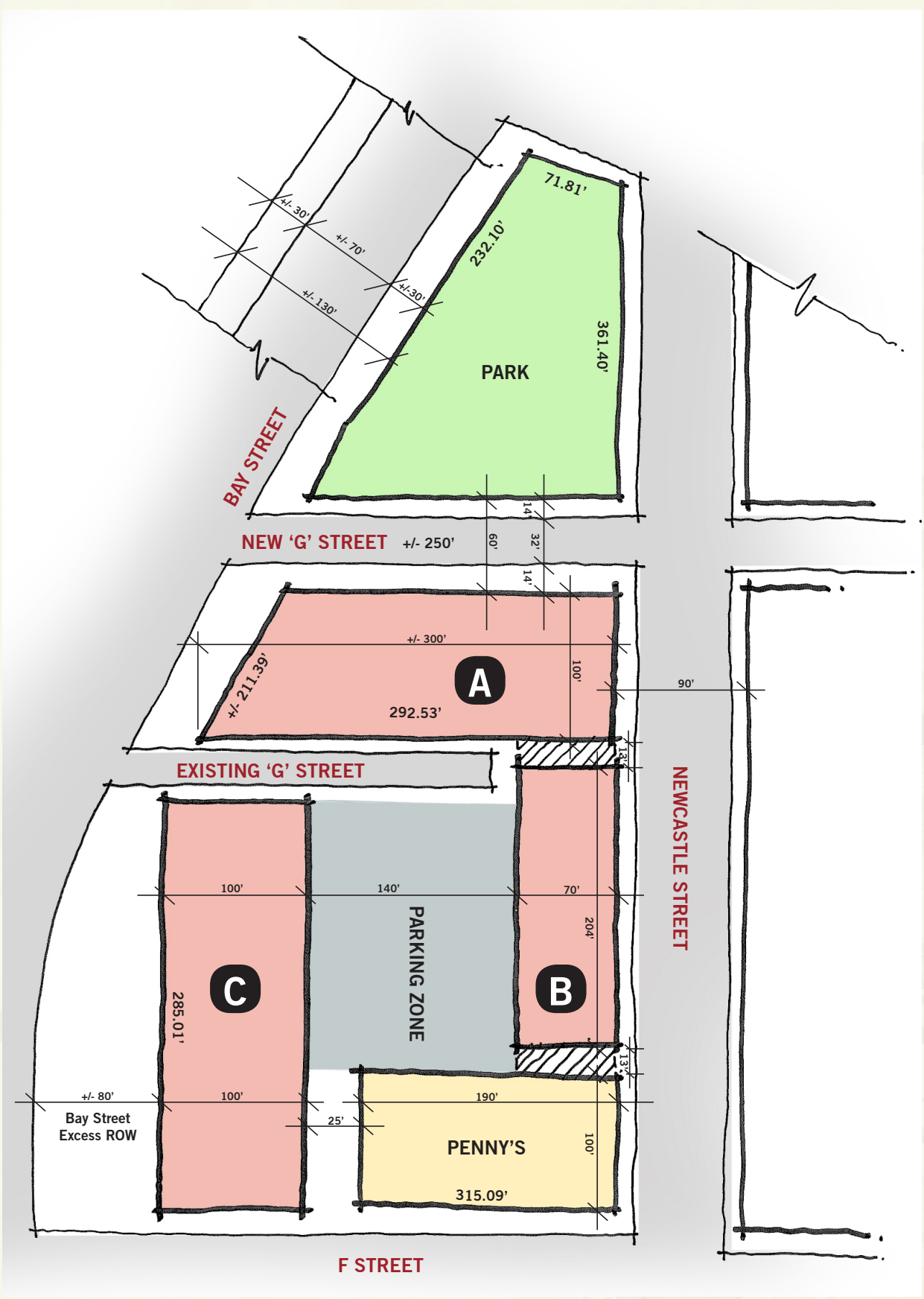
The basic components design standards for the Oglethorpe Block are the configuration dimensions and locations for each of the building blocks and Street rights of way. The building blocks are:

- The Civic Block “A” , facing the newly aligned “G” Street and containing the Conference Center and the “Chamber Group “ building
- The Commercial Block “B”, facing Newcastle Street and containing one or more attached storefront buildings with one or more (or two)occupied floors above.
- The “Mixed Use” Block “C”, facing Bay Street, which may contain such uses as retail, office, residential or a hotel. Provisions are made to allow this block to attach to the Conference Center via a bridge over the Service Corridor if all parties see a need for an attachment.

The configuration of these blocks is shown on the following two plans:

1. Regulating Plan, showing required horizontal dimensions (figure 3). The block outlines should be seen as “build to” lines as opposed to allowable setbacks. The “build to” lines must adhere to the regulations for “build-to” lines found in the Historic District Standards.
2. Regulating Volumes, showing required vertical dimensions (figure 4). The maximum allowable height for all building blocks is 60ft which corresponds to existing zoning in the block.

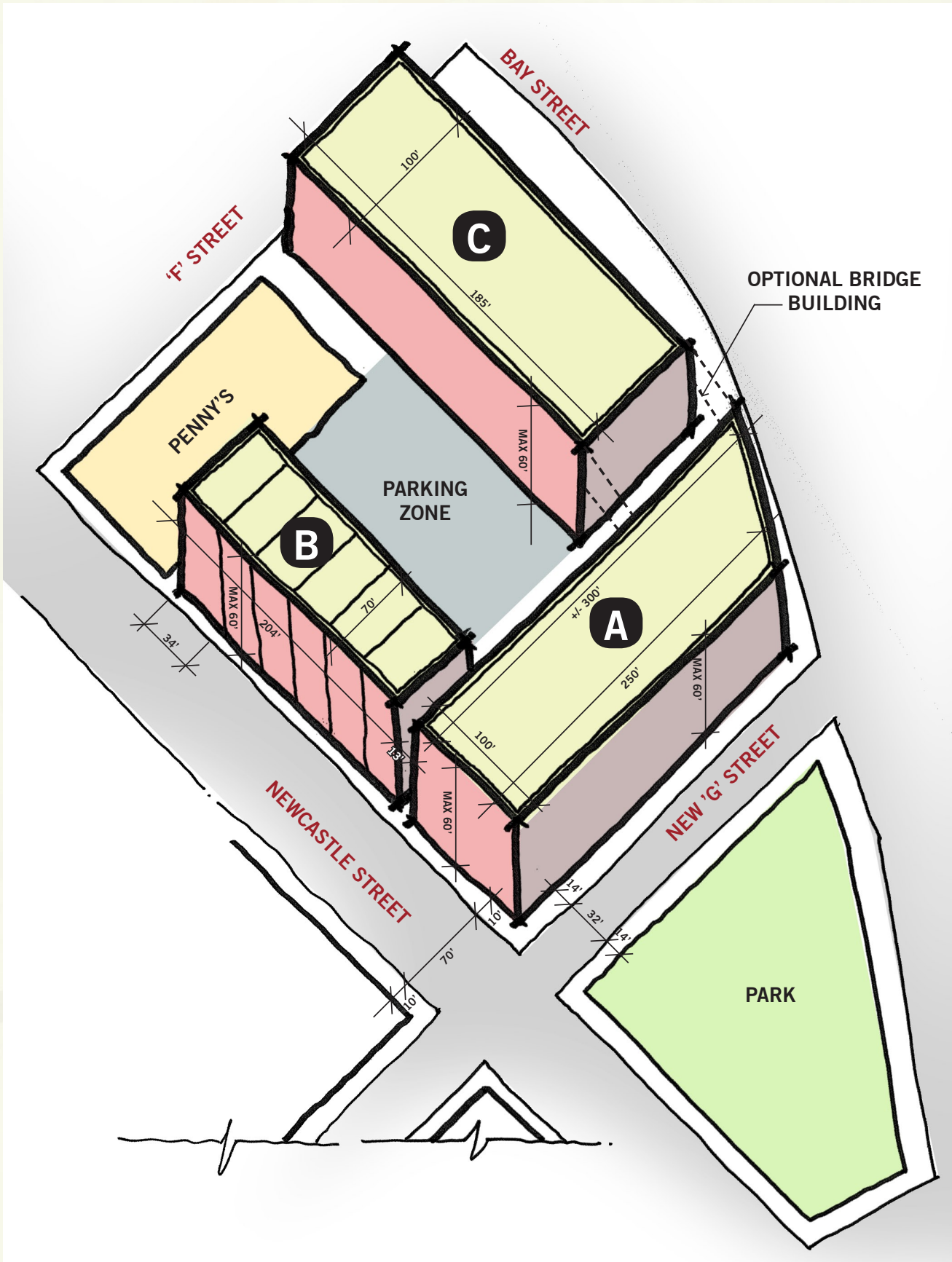




### 3 Oglethorpe Block - Regulating Plan

Not to Scale





# 4 Oglethorpe Block - Regulating Volumes

Not to Scale



# Design Standards:

## Public Zones

### A. Overview

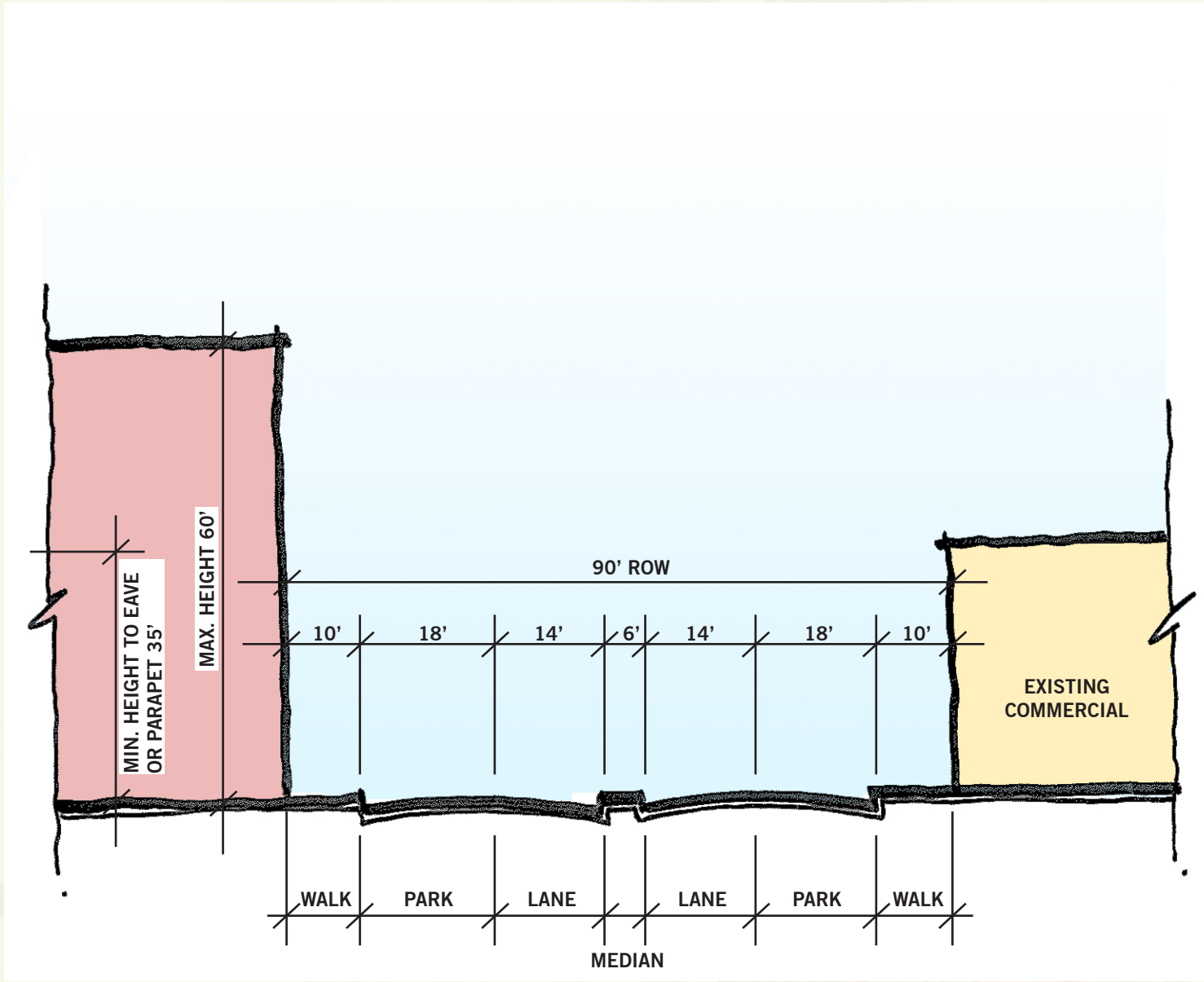
Public Zones include the public street rights of way and the Service Corridor and parking zone internal to the block. The standards show the proposed configuration of the rights of way and the relationship of the building block facades to the adjacent public right of way. These Zones are:

- Newcastle Street (figure 5), from F Street to New G Street along the redevelopment block, continuing the parking and median configuration of the rest of historic Newcastle Street
- New G Street (figure 6), from Newcastle Street to Bay Street within the redevelopment block. As mentioned, this new street is an integral part of the “Civic Zone”, providing a front door to the proposed civic buildings
- Bay Street (figure 7), from F Street to the intersection of Bay Street and Newcastle Street. The future configuration of Bay Street is still uncertain with discussions between the City, The Port Authority and GaDOT ongoing as to the future function of Bay Street and therefore its traffic volumes and characteristics. The Oglethorpe Block Study recommended a four lane facility with some of the right of way dedicated to parking for uses in the redevelopment block and public parking in general. This should be understood as but one alternative for the Bay Street right of way at this time.
- Service Street and Parking Zone (figure 8). Service to the interior of the redevelopment block is to be provided along a reconfigured service corridor lying wholly within the existing G Street right of way. This corridor will serve all three of the redevelopment building blocks and the parking zone, the phasing strategy for which has been discussed previously.

### B. Streetscape Standards

These design standards do not include standards and design specifications for streetscape elements such as paving, landscaping, or elements such as lighting, seating and signage. It is recommended the standards along Newcastle Street be a continuation of those in place and previously installed along the rest of this historic Corridor. However, while some standards could be similar along New G Street and Bay Street, each of these two corridors is different and unique and should have streetscape elements that are consistent with the scale and character of each of these corridors. In particular, the design for New G Street is decidedly civic in nature and should be developed with the design of the Civic buildings and the redesign of Oglethorpe Park to constitute the “Gateway” role of the area in the context of Brunswick overall.

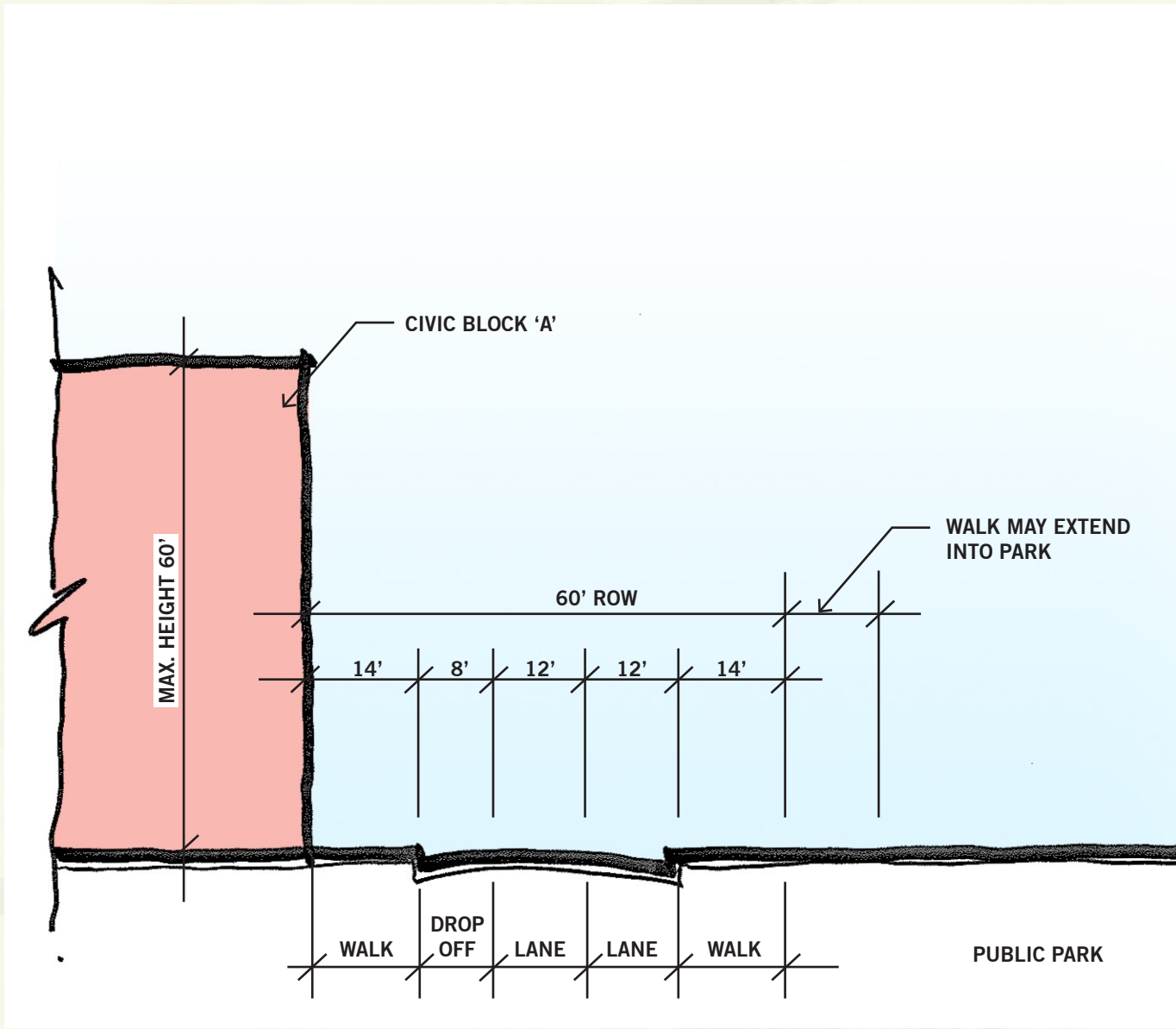




# 5 Street Design Standards - Newcastle Street

Scale 1" = 20'

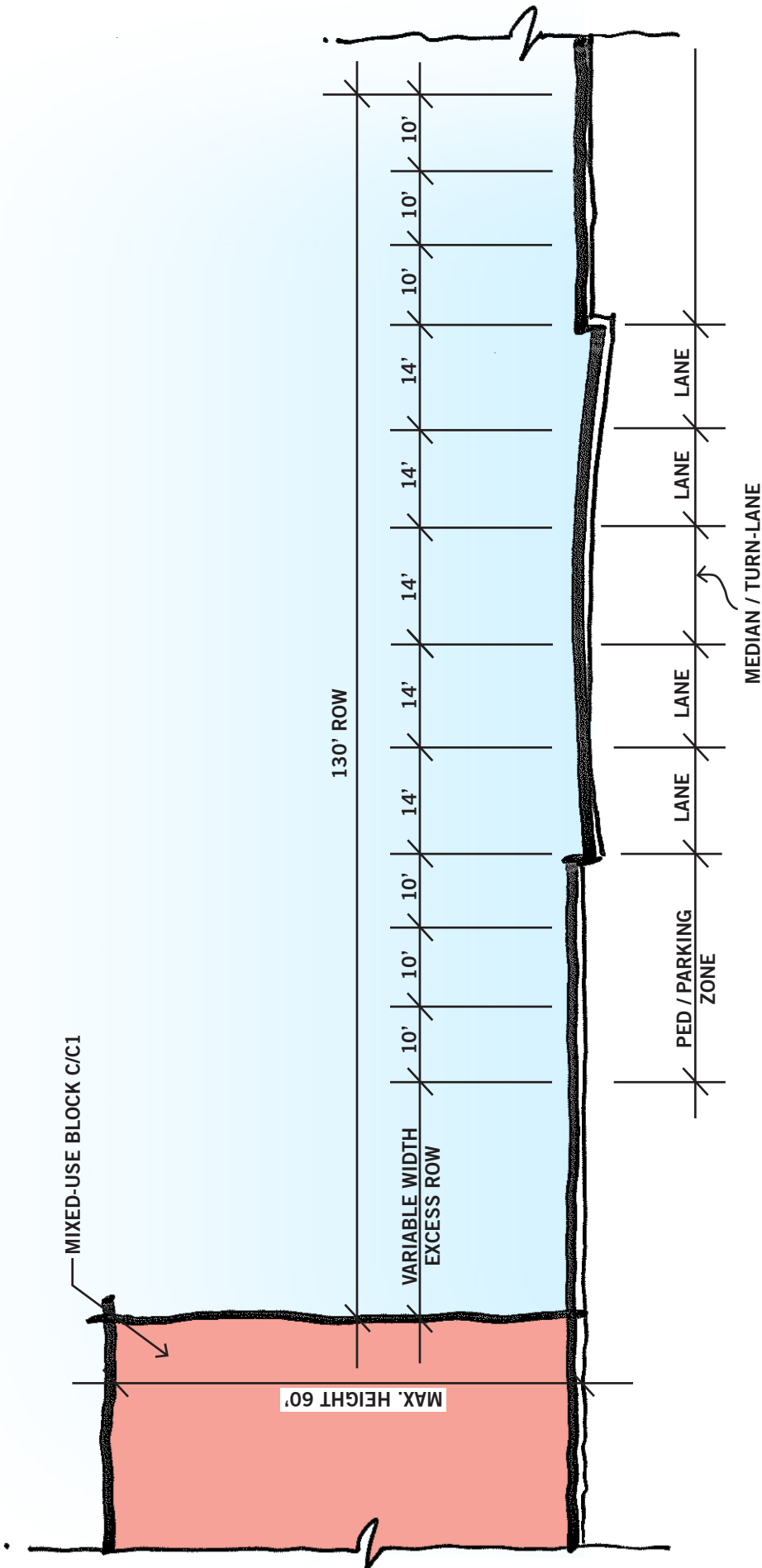




# 6 Street Design Standards - New 'G' Street

Scale 1" = 20'



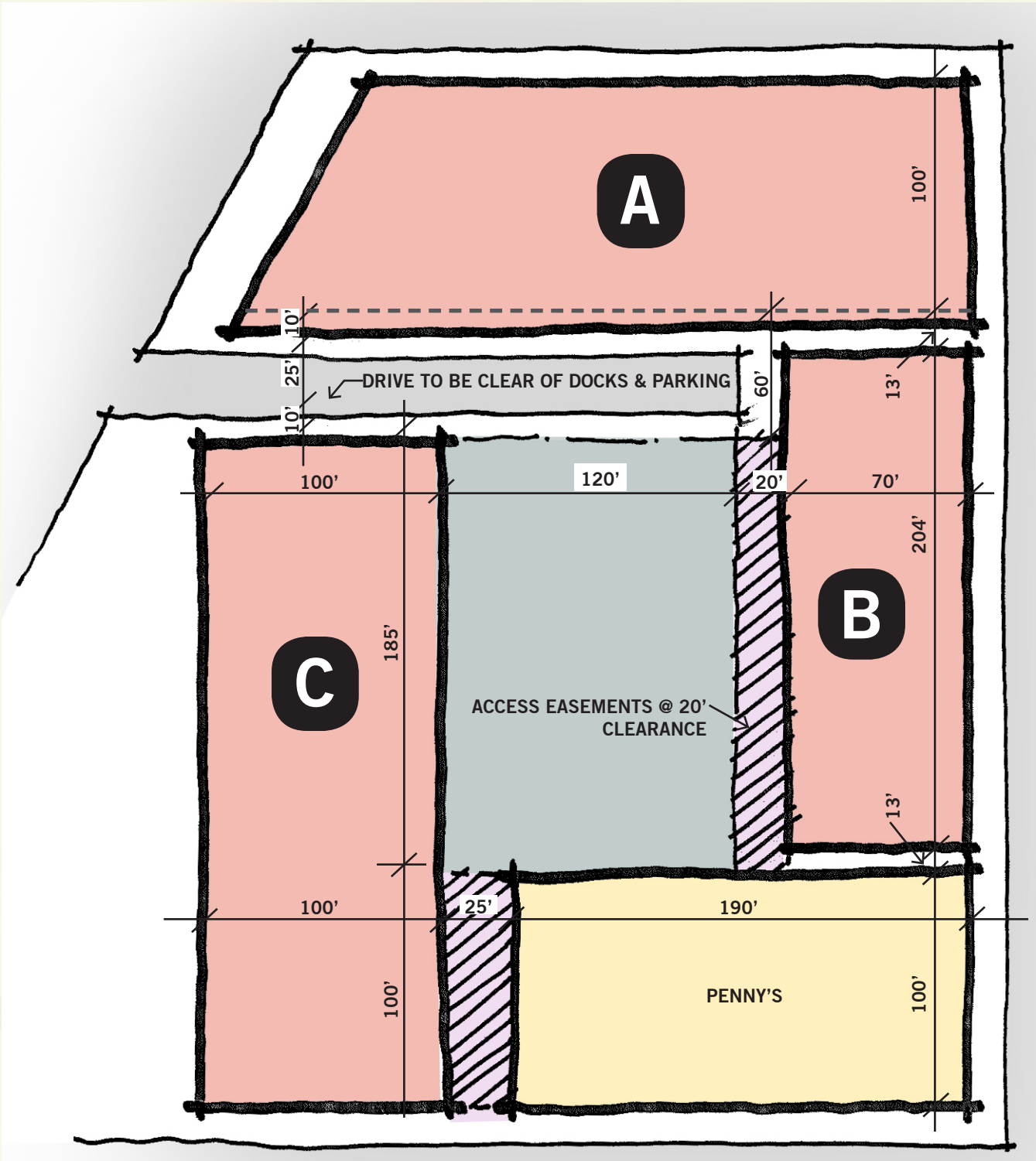


**7 Street Design Standards - Bay Street**

Scale 1" = 20'







# 8 Standards for Parking Zone & Service Drive

Scale 1" = 60'



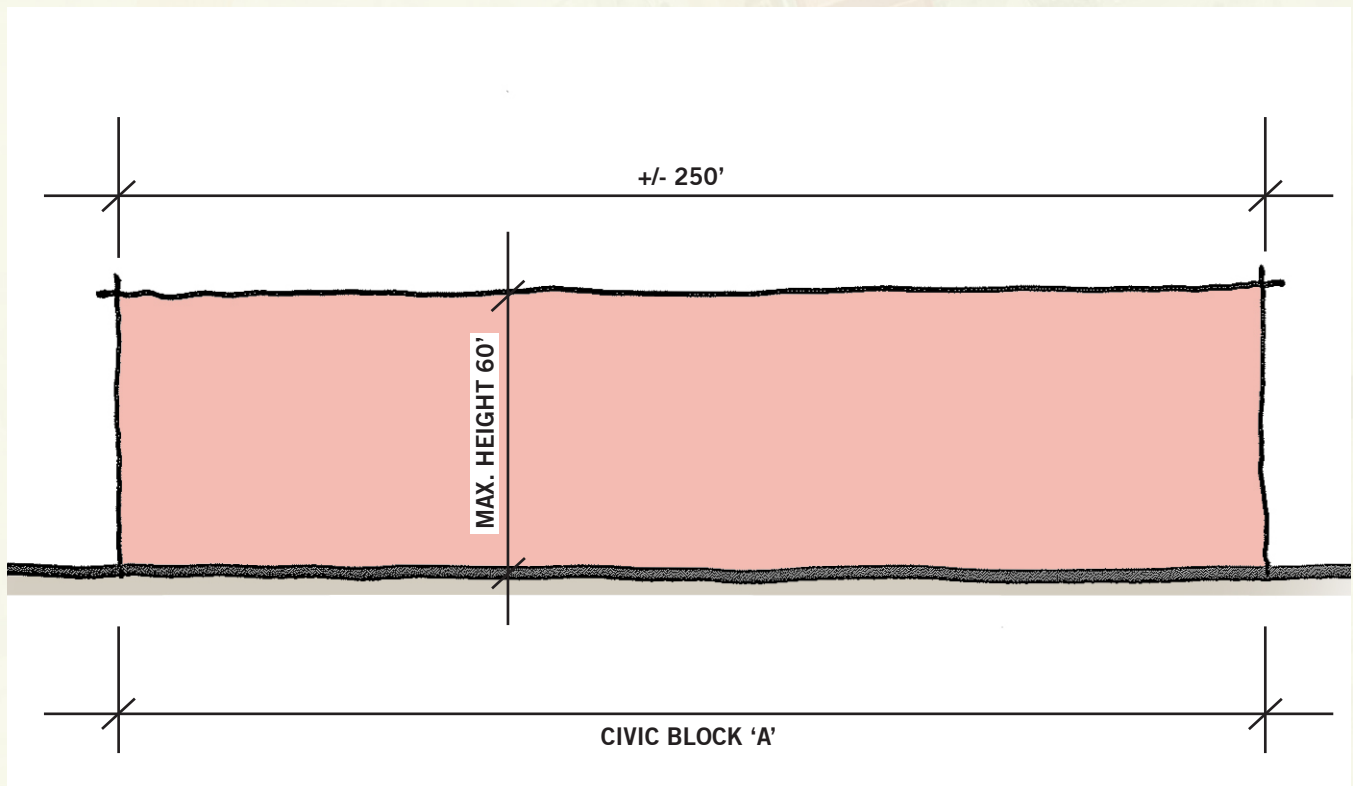
# Design Standards: Building Development Blocks

## A. Overview

The three building blocks in the redevelopment plan vary substantially in the uses they contain, in the scale and character of the environment they front on and in the differences in the public and/or private nature of each block within the urban landscape. Each therefore should be approached with appropriate design responses to all of these variables. However, they should still exhibit some degree of unity, particularly with respect to scale and height, as they are adjacent to one another on one of the most important blocks in downtown Brunswick. Therefore, design standards are approached with some variation for each of the blocks as follows:

- The Civic Block “A” (figure 9). Allowable dimensions and heights are shown in the regulating plans and in more detail on Figure 9. It is recommended that the specific design standards be left as flexible as possible to allow design latitude in creating a signature “civic gateway” expression that has been discussed previously
- The Commercial Block “B” (figure 10). This block will consist of 6 (7) attached shopfront buildings, each having a 34ft frontage on Newcastle Street. Allowable dimensions and heights are shown in the regulating plans and in more detail on Figure 10. It is recommended that specific design standards be strictly in keeping with the Historic District and the proposed Shopfront Standards and following the spirit and scale of the historic shopfront building vocabulary all along Newcastle Street. Additional standards for this particular block are shown in more detail on figure 11 and are proposed to be conveyed as covenant restrictions with the sale of individual properties. Formost among these is a 35ft minimum height requirement to the eave or parapet along the build-to line to maintain the scale along Newcastle and within the Oglethorpe Block.
- The “Mixed Use” Block “C” (figure 12). Allowable dimensions and heights are shown in the regulating plans and in more detail on Figure 12. It is recommended the specific design standards be left very flexible for this building block due to the general scale of Bay Street and the unknown use(s) which this block will accommodate.

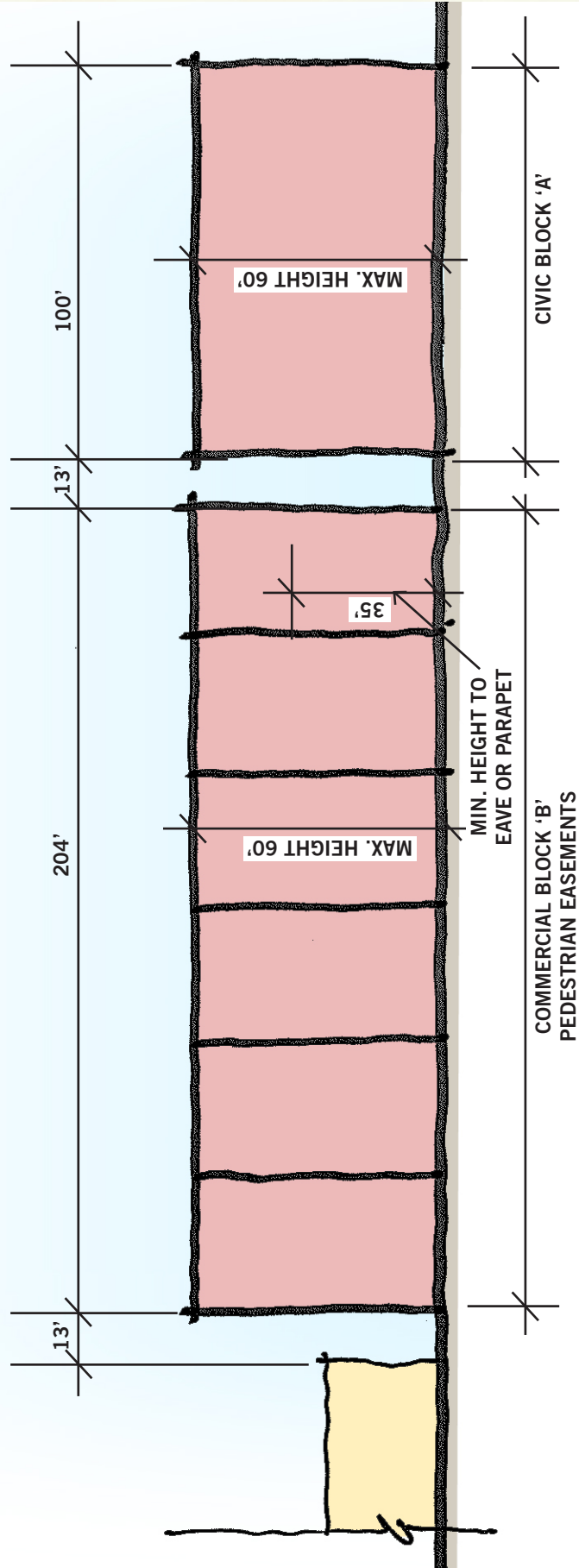




# 9 Facade Standards - New 'G' Street

Not to Scale

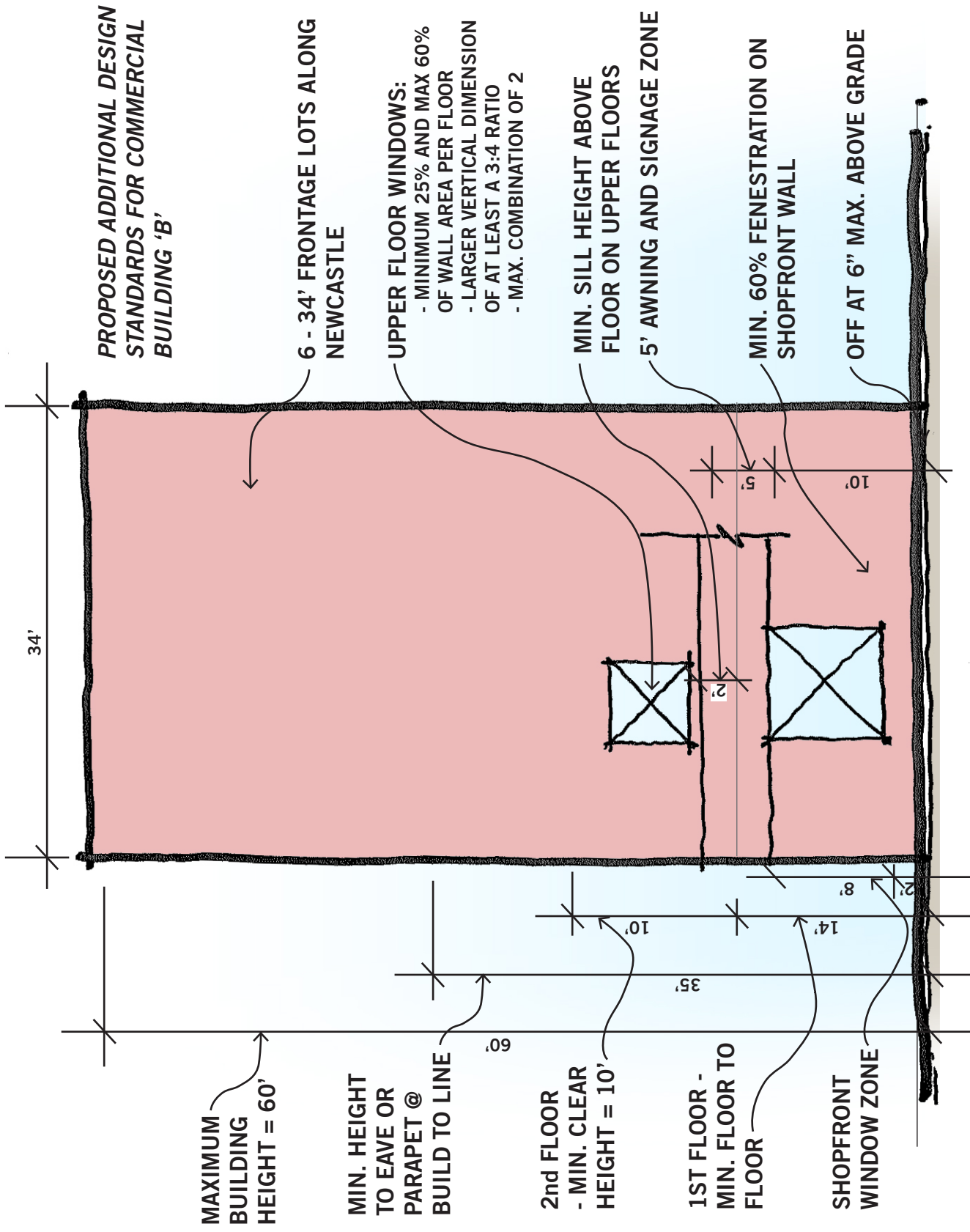




# 10

Facade Standards - Newcastle Street  
Not to Scale

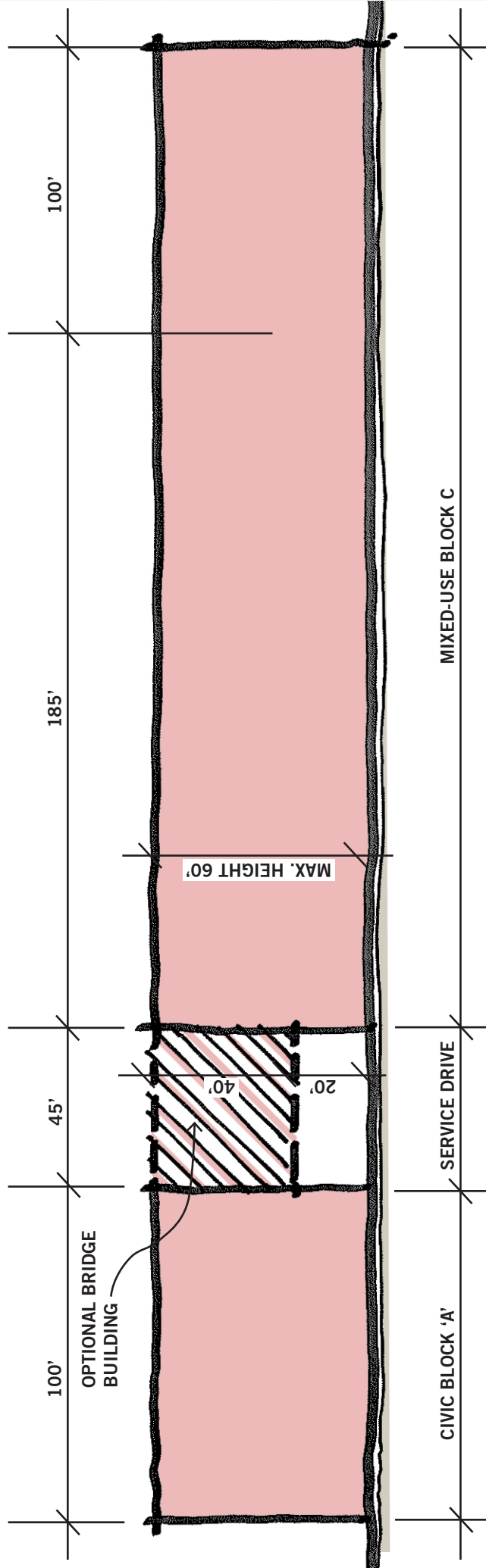




# 11 Facade Design Standards - Newcastle Street Commercial Building Block 'B'

Not to Scale





# 12 Facade Design Standards - Bay Street

Not to Scale



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# Appendix

**Appendix A - Proposed Shopfront Design Standards**

**Appendix B - Existing Conditions (Site Photos)**

**Appendix C - Site Survey from the 1976 “Urban Renewal Plan”**

**Appendix D - Site Utility Survey from 2004 “Oglethorpe Block Study”**



# Appendix A

Proposed Shopfront Design Standards





# Downtown Brunswick PD-OB Zone

## Shop Front Buildings

### Illustrations and Statement of Intent

*Note: these are provided as illustrations of intent. The illustrations and statements on the following pages are advisory only and do not have the power of law.*

*Shop front* buildings are the basic building block of every American Main Street. Designed to fire and foster an active street life, their ground floor fronts have large windows to encourage the connection between the commercial activity within and the public life walking or driving by. Ground-floor uses should be flexible to accommodate a variety of retail or office uses. Upper-story uses are best as residential apartments or condos, but they may be offices or workshops on very busy streets. Above-shop residences have proven to be good quality and affordable places for downtown living.



# Shop Front Buildings

## Placement Standards

<p><b>Height</b></p>		<p>The building shall be no more than 60 feet to its eaves. The ground floor elevation shall be no more than 6 inches above the fronting sidewalk elevation. No less than 80% of the ground floor shall have at least 12 feet clear height. No less than 80% of the second storey shall have at least 10 feet clear height. Buildings shall be between 2 &amp; 4 stories.</p>
<p><b>Siting</b></p>		<p>The Street Façade shall be Built-To not less than 100% of the street frontage along Newcastle Street and 80% along Bay Street, F and G streets. The maximum lot width/ building Street Frontage is 34 feet along Newcastle Street. There are no required side setbacks.</p>
<p><b>Elements</b></p>		<p>The primary street façade shall have no less than 60% glass fenestration (between 2 and 10 feet above the fronting sidewalk). Awnings and Overhangs of at least 6' projection are encouraged. Shop signs (not more than 1 foot vertical x 3 foot horizontal and minimum 7' clear height above the sidewalk may be hung from such an overhang.</p>
<p><b>Uses</b></p>		<p>The Ground floor shall be only commercial uses: Retail, Office, Restaurant and Artisanal Workshops (where there is a storefront office for customer entry). The second floor may be either residential or office use (including small hotel and bed &amp; breakfast operations.) Parking, garages, and accessory uses shall be placed to the rear of the lot. Parking shall be no closer than 25 feet to any Street Frontage and separated from an adjacent different use by a Garden Wall.</p>

# Shop Front Buildings

## Architectural Standards

### STANDARDS FOR BUILDING WALLS (WHERE CLEARLY VISIBLE FROM THE STREET)

**Materials:** The following materials are permitted.

- Red Brick and tile masonry
- Pre-cast masonry (for trim and cornice elements only)
- Gypsum Reinforced Fiber Concrete (GFRC—for trim elements only)
- Metal (for beams, lintels, trim elements and ornamentation only)
- Split-faced block (only for piers, foundation walls and chimneys)

**Configurations and Techniques:** The following configurations and techniques are permitted.

#### *Walls*

- Wall openings shall not span vertically more than ONE STORY.
- Wall openings shall correspond to interior space and shall not span across building structure such as the floor structural and mechanical thickness.
- Wall materials shall be consistent horizontally (i.e. joints between different materials must be horizontal and continue around corners) except for chimneys and piers.
- Material changes shall be made within a constructional logic—as where an addition (of a different material) is built onto the original building.
- Brick, Block and Stone
  - Must be properly detailed and in appropriate load-bearing configurations.
- No “tabby” finishes

### STANDARDS FOR ROOFS AND PARAPETS (Where clearly visible from the Street)

**Materials:** The following materials are permitted.

- Clay or concrete (faux clay)
  - Tile (barrel or flat roman)
  - Slate (equivalent synthetic or better)
  - Metal (standing seam 5-v crimp, equivalent or better)
  - Dimensional Asphalt shingles
  - Cornices and soffits may be a combination of wood, vinyl, and/or metal
- Configurations and Techniques:** The following configurations and techniques are permitted.

**Configurations and Techniques:** The following configurations and techniques are permitted.

#### **PARAPET Roofs** (Cornice, Entablature, and Coping Standards) Required

#### **Cornices and Other Features**

- Skylights and roof vents are permitted only on the roof plane opposite the primary STREET or RBL or when shielded from STREET view by the building’s PARAPET wall.
- Overly elaborate, “postmodern” and/or “high-tech” designs are discouraged. However,



# Shop Front Buildings

## Architectural Standards

ornamentation which contributes to the character of the building is encouraged. Consult the Historic Preservation Board for appropriate configurations.

- Green roof technologies are encouraged. Vegetative cover should be considered for flat roofs and solar panels should be considered for integration into pitched roof structures.

### STANDARDS FOR WINDOWS & DOORS (Where Clearly Visible From the Street)

**Materials:** The following materials are permitted.

- Windows shall be of wood or clad wood.
- Window glass shall be clear, with light transmission at the GROUND STORY at least 90 percent and for the upper stories 75 percent. Specialty windows may utilize stained, opalescent or glass block (one per façade maximum).
- Window screens shall be black or gray
- Screen frames shall match window frame material
- Doors shall be of wood, clad wood or steel.

**Configurations and Techniques:** The following configurations and techniques are permitted.

- The following requirements apply to all windows:
  - Windows must be taller than they are wide by a ratio of 3:4
  - Windows may be ganged horizontally (maximum 2 per group) if each grouping is separated by a mullion, column, pier or wall section that is at least 7 inches wide.
  - Windows shall be no closer than 30 inches to Building Corners (Excluding Bay windows and where the Building Corner is also a Block Corner.)
  - Exterior shutters, if applied, shall be sized and mounted appropriately for the window (one-half the width), even if inoperable.
- The following requirements apply to all upper-story windows:
  - Windows shall be double-hung.
  - Fixed windows are permitted only as a component of a system including operable windows within a single wall opening.
  - Egress windows may be installed according to the appropriate building code.
- The following requirements apply to shop front (ground floor) windows and doors:
  - Single panes of glass not larger than 6 feet in height by 4 feet wide.
  - Ground floor windows shall not be made opaque by window treatments (excepting operable sunscreen devices within the conditioned space) and shall allow a minimum 60% of surface view into the building for a depth of at least 20 feet.
- Doors
  - Double-height entryways (those that span more than one story) are not allowed.
  - Doors shall not be recessed more than 3 feet behind the shop-front windows and in any case, shall have a clear view and path to a 45-degree angle past the perpendicular from each side of the door.
  - Roll-down security gates and doors are prohibited.



## STANDARDS FOR SIGNAGE (Where Clearly Visible from the street)

### General:

- Wall signs are permitted within the area between the second story floor line and the first floor ceiling, within a horizontal band not to exceed 2 feet in height. In no case shall this band be higher than 18 feet or lower than 12 feet above the adjacent sidewalk.
- Letters shall not exceed 18 inches in height or width and 6 inches in relief. Signs shall not come closer than 2 feet to an adjacent common lot line.
- Company logos or names may be placed within this horizontal band or placed or painted within the Ground Floor or second story office windows. Company logos or names shall not be larger than a rectangle of 8 square feet.
- A masonry or bronze plaque bearing an owner's or building's name may be placed in the building's cornice/parapet wall or under the eaves, and above the upper story windows. Any such plaque shall be no larger than a rectangle of 8 square feet.
- Street addresses may be placed at street entry doors using 6 inch tall, non-cursive type lettering. Such letter shall be between 6 feet and 10 feet above the grade.
- Blade type shop signs (not more than 12 inches vertical by 3 feet horizontal and minimum 7 feet clear height above the sidewalk) are encouraged and may be hung from an overhang or awning. Blade signs shall not be internally illuminated and the company name or logo may occupy no more than one-half of the square footage within the blade sign. Creative art, graphics or materials is encouraged in the area of the blade sign not containing the company name or logo. Blade signs shall be permitted in addition to the permitted square footage of signage affixed to the façade of the building.
- Prohibited Signs: Billboards, canopy signs, marquees, any kind of animation, roof signs, No flashing, traveling, animated or intermittent lighting shall be on the exterior of any building whether such lighting is of temporary or long-term duration.
- External lighting directed towards signage that is not internally lit is permitted. The energy efficiency of lighting should be considered.

### Awnings & Overhangs:

When an awning or overhang is incorporated into a building, the following requirements must be met:

- Minimum 10 feet clear height above sidewalk, minimum 6 feet depth out from the building façade (maximum to curb or tree-planting strip/furniture zone, whichever is closer).
- Canvas cloth or equivalent (no shiny or reflective materials, metal or glass)
- No internal illumination through the Awning/overhang.
- Lettering on awnings is limited to 5 inches tall on vertically hanging fabric at the curb side of awning.
- No one-quarter cylinder configurations.

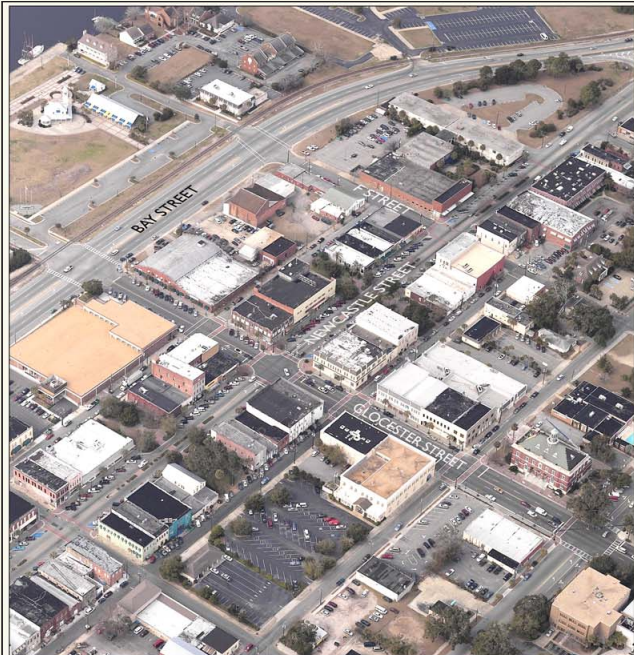


# Appendix B

Existing Conditions  
(Site Photos)



# EXISTING CONDITIONS



VIEW LOOKING NORTH-WEST



VIEW LOOKING SOUTH-EAST



7 F STREET (LOOKING NORTH)



8 F STREET (LOOKING SOUTH)



6 F STREET (LOOKING SOUTH)



5 NEWCASTLE STREET (LOOKING WEST)



4 NEWCASTLE STREET (LOOKING WEST)



2 NEWCASTLE STREET (LOOKING EAST)



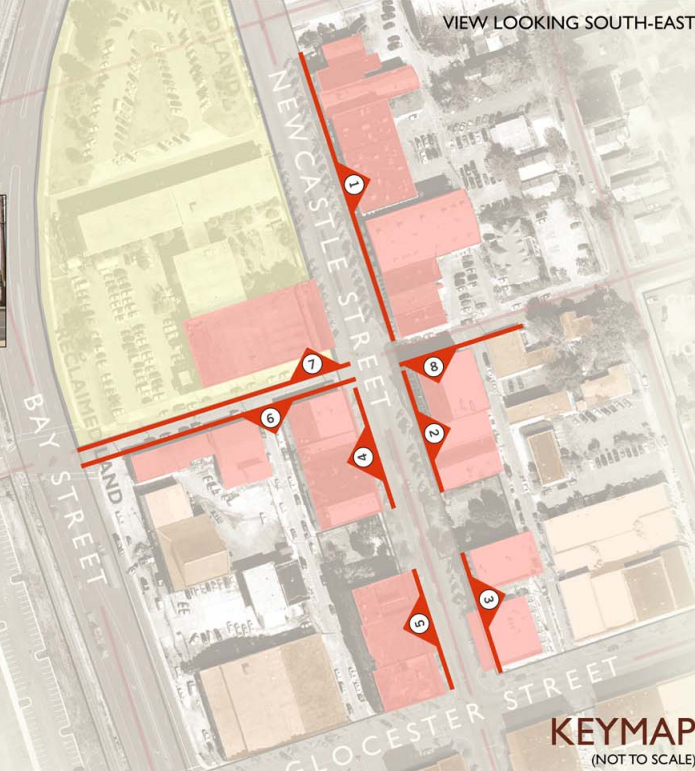
3 NEWCASTLE STREET (LOOKING EAST)



1 NEWCASTLE STREET (LOOKING EAST)

VISUAL FOCAL POINT

DOWNTOWN GATEWAY



KEYMAP  
(NOT TO SCALE)

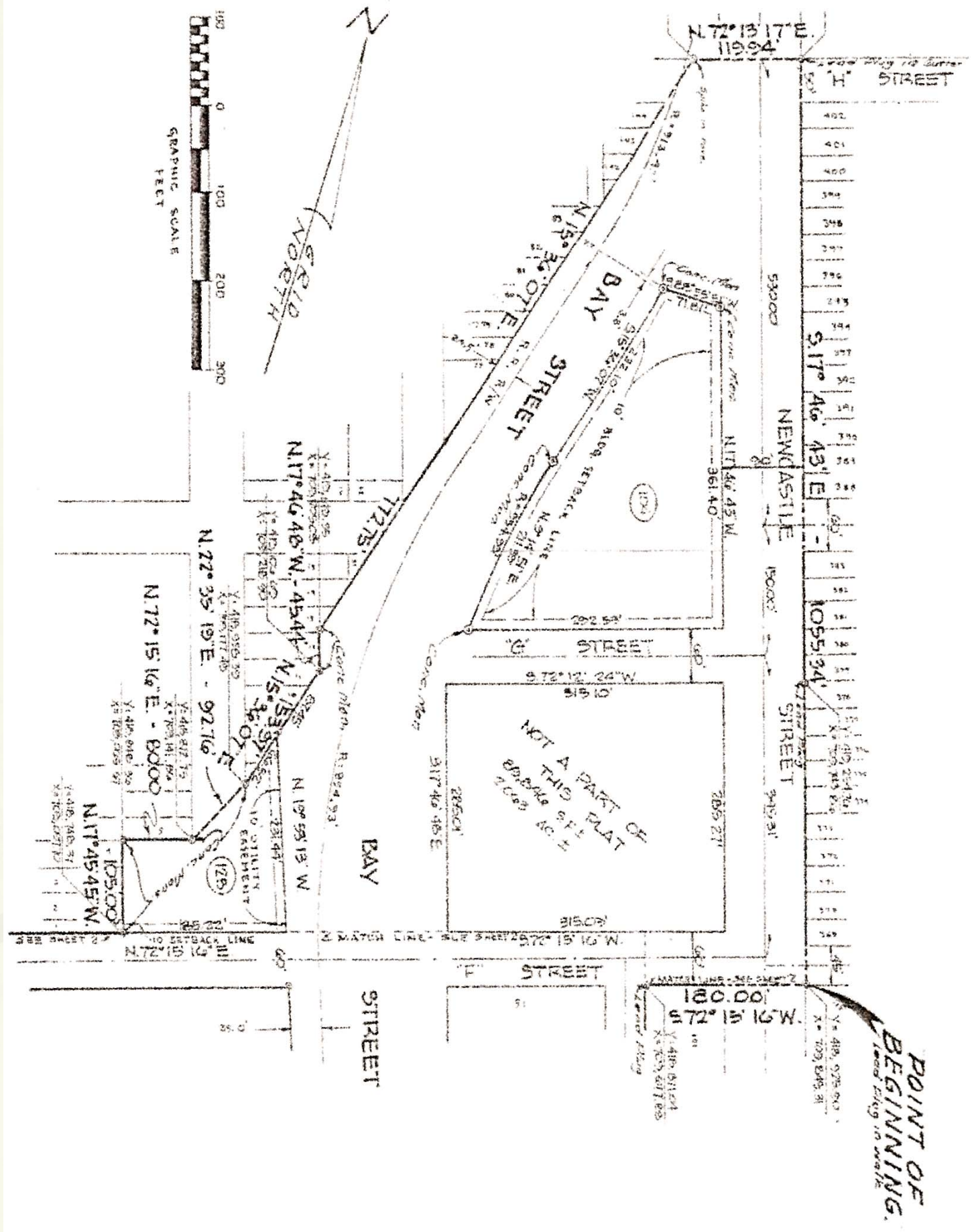


# Appendix C

Site Survey from the 1976 "Urban Renewal Plan"



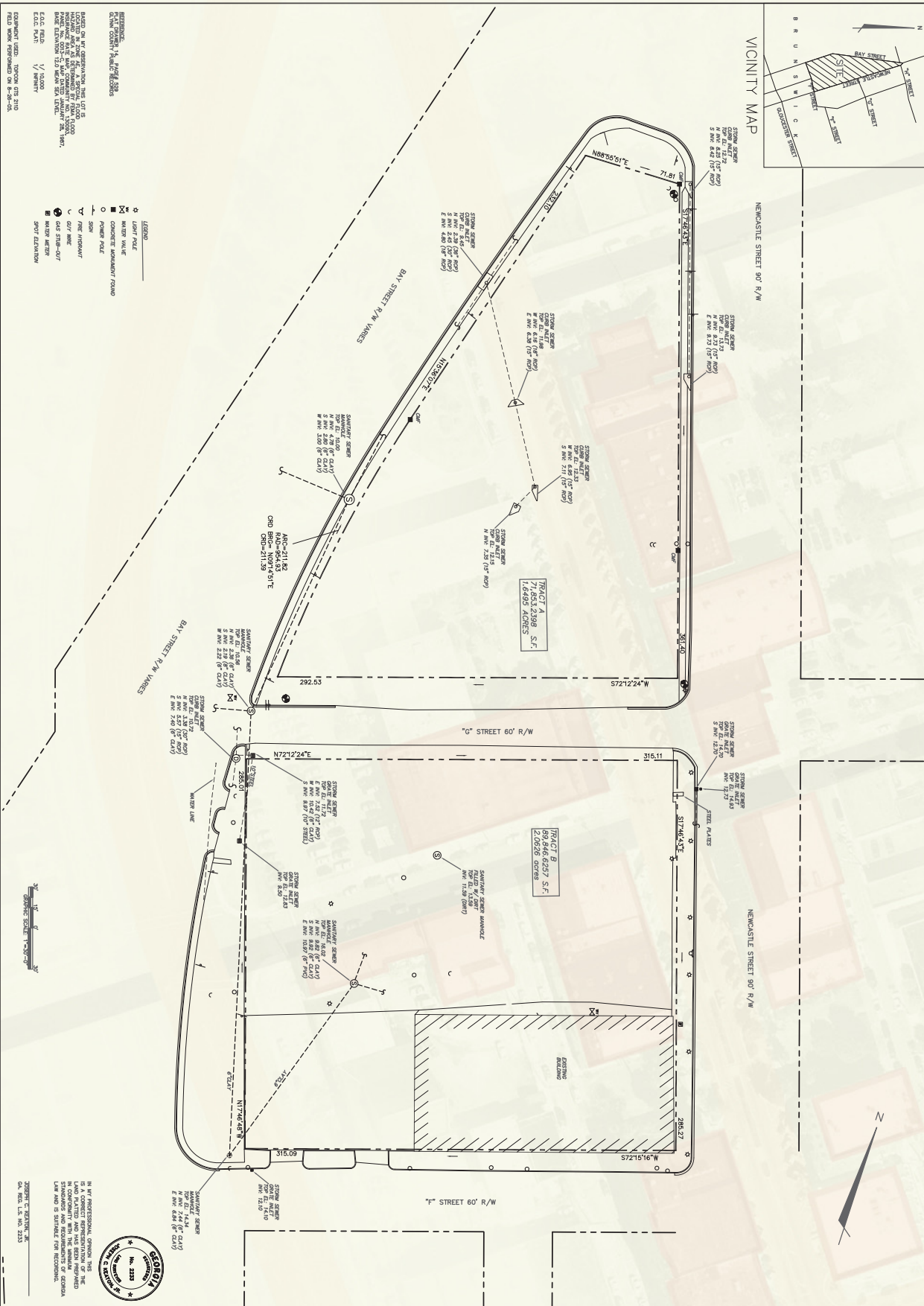




# Appendix D

Site Utility Survey from 2004 "Oglethorpe Block Study"





A SURVEY OF "THE  
 OGLETHORPE BLOCK PROJECT",  
 26TH G.M. DISTRICT, LYNN  
 COUNTY, BRUNSWICK, GEORGIA.

SURVEYED FOR:  
 BLUEPRINT BRUNSWICK

EMC ENGINEERING  
 SERVICES, INC.

DESIGN: JMC  
 GRAPHICS: JMC  
 REVIEW: JMC  
 DATE: 8-31-06  
 SCALE: 1"=20'  
 PROJECT: 05-106

REVISIONS:

No.	BY	DATE

BRUNSWICK OFFICE  
 3181 Cypress Mill Road  
 Brunswick, Georgia 31525  
 Phone: (912) 265-7636  
 Fax: (912) 265-4886